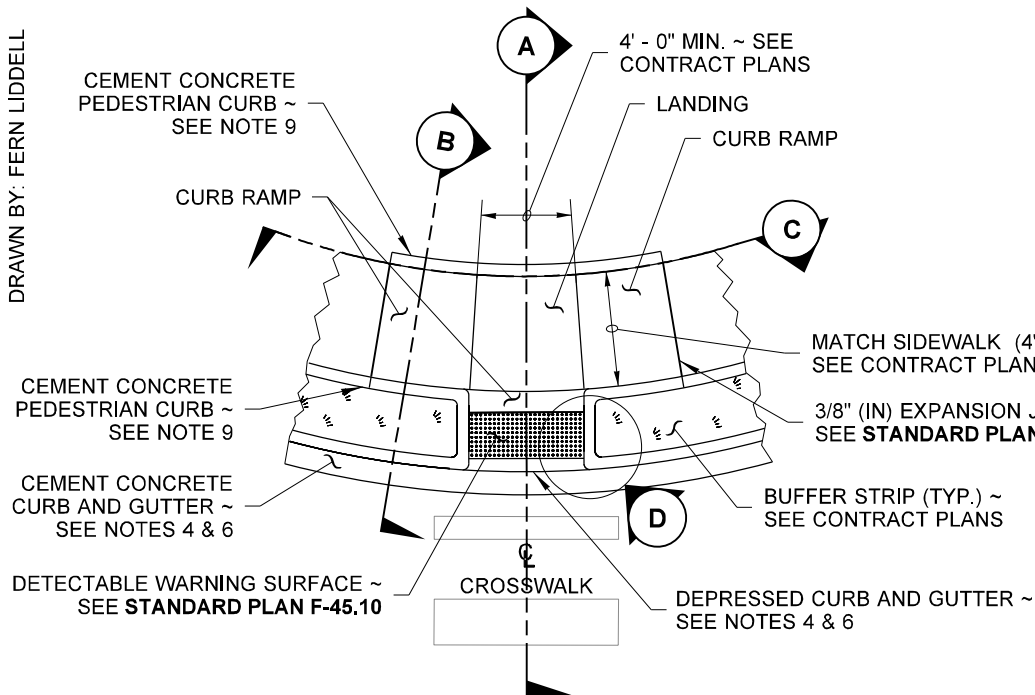
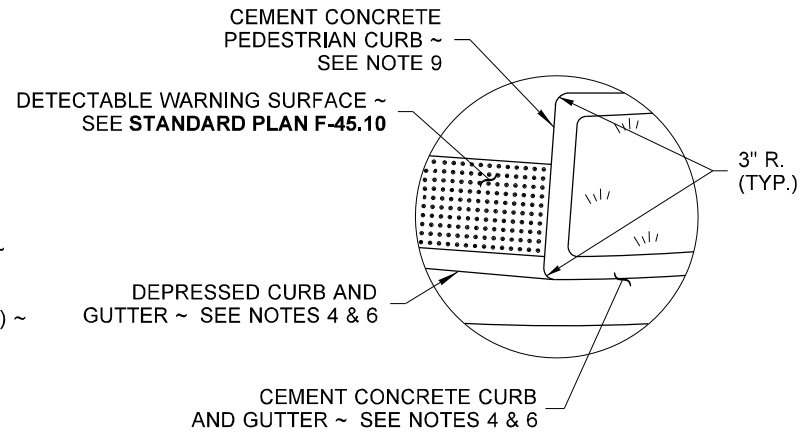


DRAWN BY: FERN LIDDELL



PLAN VIEW  
TYPE COMBINATION  
WITH BUFFER



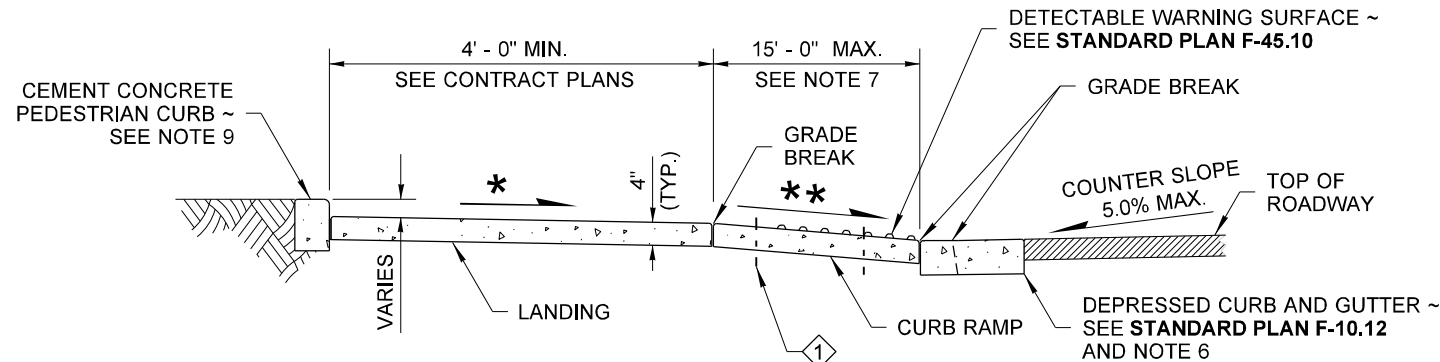
DETAIL D  
CURB RADIUS DETAIL

## NOTES

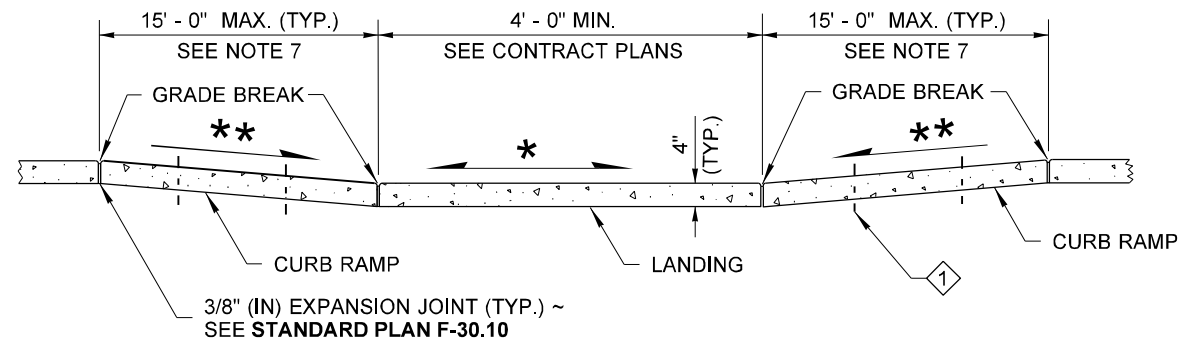
- At marked crosswalks, the connection between the curb ramp and the roadway must be contained within the width of the crosswalk markings.
- Where "GRADE BREAK" is called out, the entire length of the grade break between the two adjacent surface planes shall be flush.
- Do not place Gratings, Junction Boxes, Access Covers, or other appurtenances on any part of the Curb Ramp or Landing, or in the Depressed Curb and Gutter where the landing connects to the roadway.
- See Contract Plans for the curb design specified. See **Standard Plan F-10.12** for Curb, Curb and Gutter, Depressed Curb, Gutter and Pedestrian Curb details.
- See **Standard Plan F-30.10** for Cement Concrete Sidewalk Details. See Contract Plans for width and placement of sidewalk.
- The Bid Item "Cement Concrete Curb Ramp Type \_\_\_" does not include the adjacent Curb, Curb and Gutter, Depressed Curb and Gutter, Pedestrian Curb, or Sidewalks.
- The Curb Ramp length is not required to exceed 15 feet (unless otherwise shown in the Contract Plans). When applying the 15-foot max. length, the running slope of the curb ramp is allowed to exceed 8.3%. Use a single constant slope from bottom of ramp to top of ramp to match into the sidewalk over a horizontal distance of 15 feet. Do not included the abutting landing in the 15-foot max. measurement. When a ramp is constructed on a radius, the 15-foot max. length is measured on the inside radius along the back of the walkway.
- Curb Ramps and Landings shall receive a broom finish. See **Standard Specifications 8-14**.
- Pedestrian Curb may be omitted if the ground surface at the back of the Curb Ramp and/or Landing will be at the same elevation as the Curb Ramp or Landing and there will not be material to retain.

## LEGEND

- SLOPE IN EITHER DIRECTION
- \* 1.5% OR FLATTER RECOMMENDED FOR DESIGN/FORMWORK (2% MAX.)
- \*\* 7.5% OR FLATTER RECOMMENDED FOR DESIGN/FORMWORK (8.3% MAX.)



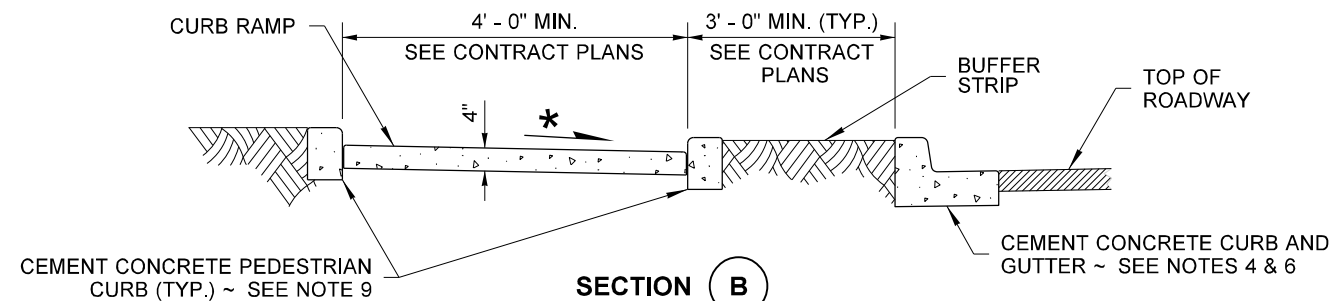
SECTION A



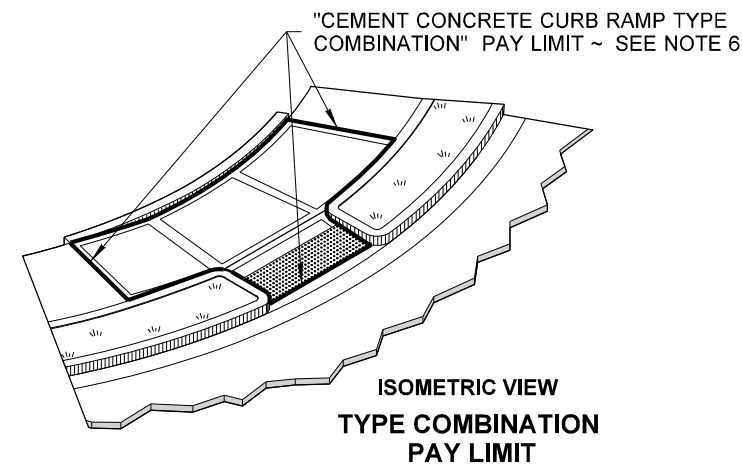
SECTION C

(ALONG INSIDE RADIUS AT BACK OF WALKWAY)

1 CONTRACTION JOINT (TYP.) ~ SEE **STANDARD PLAN F-30.10** FOR CURB RAMP LENGTHS GREATER THAN 8' - 0" PROVIDE CONTRACTION JOINT EQUALLY SPACED 4' - 0" MIN. OC.



SECTION B



ISOMETRIC VIEW  
TYPE COMBINATION  
PAY LIMIT



*[Signature]*

Zeller, Scott  
Jun 24 2016 7:20 AM

## COMBINATION CURB RAMP

## STANDARD PLAN F-40.14-03

SHEET 1 OF 1 SHEET

APPROVED FOR PUBLICATION

*Carpenter, Jeff*

Carpenter, Jeff  
Jun 29 2016 2:28 PM

STATE DESIGN ENGINEER



Washington State Department of Transportation

## SUPPLEMENTAL TO STANDARD PLANS F-40.12-03, F40.14-03, F40.15-04, F40.16-03

### **Modify the Standard Plan as follows:**

#### Notes:

1. Construct a 4" thick layer of compacted CSTC under all concrete.
2. Delete reference to expansion joints in the Standard Plan. Where expansion joints are shown on the Standard Plan construct a contraction joint, except where the new concrete abuts existing construct a construction joint.
3. A raised edge per WSDOT Standard Plan F-30.10-03 may be constructed instead of pedestrian curb.
4. The depressed curb and gutter abutting the width of the curb ramp shall have 0" height.
5. Pay limits for curb ramps are modified to include the pedestrian curb/raised edge, where shown in the plans. The measurement of the area of pedestrian curb/raised edge included in the pay limits will be limited to the surface area of the top of the pedestrian curb/raised edge. Modify note 6 to delete "Pedestrian Curb".