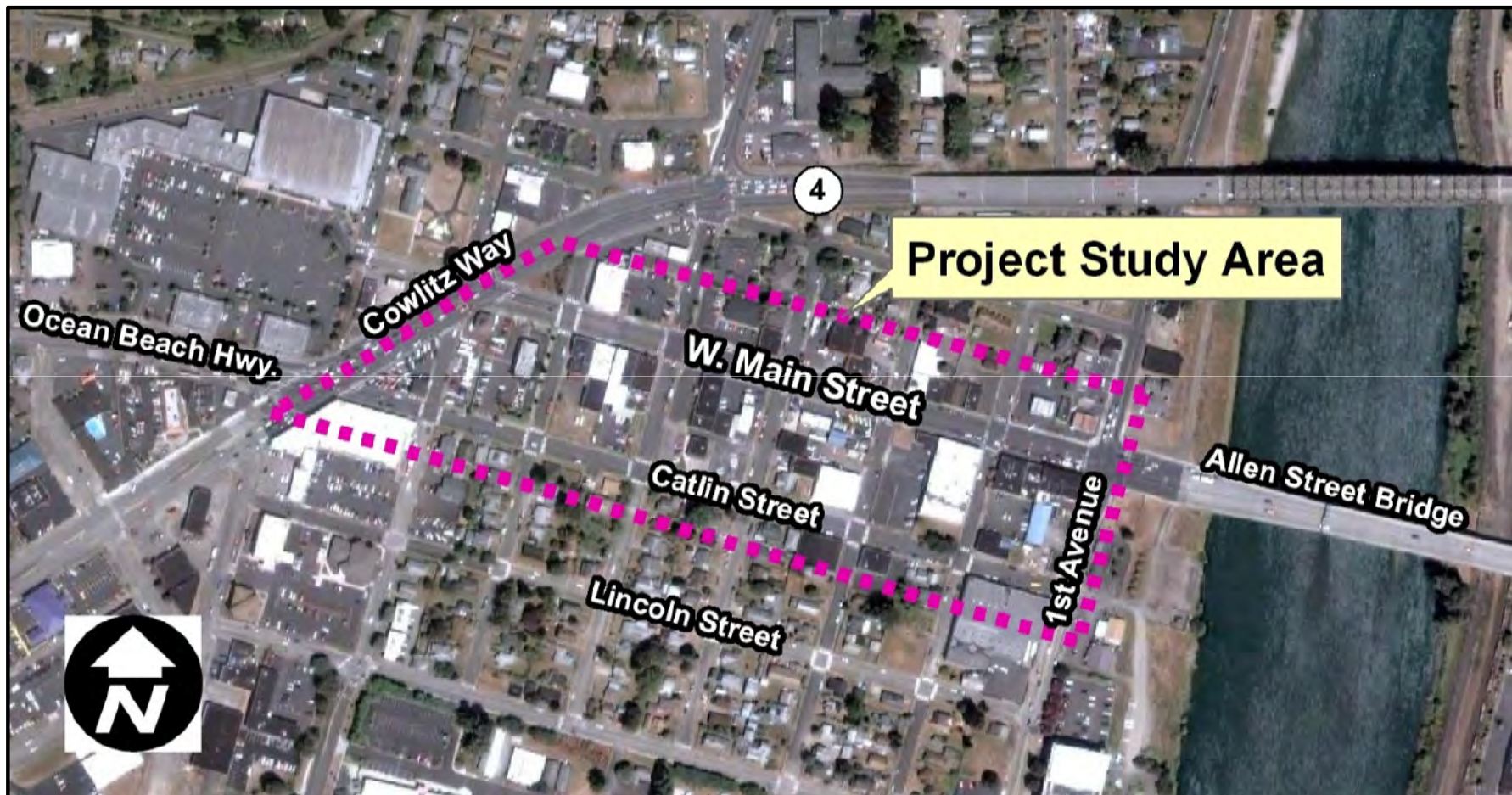


# West Main Street Realignment Project

City Council – November 3, 2009



# West Main Realignment Project Area



# Goals

- **Realign Main Street to meet Ocean Beach Highway**
- **Reduce traffic congestion at major corridor intersections**
- **Create a western gateway feature for Kelso**
- **Provide an attractive, cohesive boulevard streetscape**
- **Improve pedestrian safety**
- **Encourage a network of parallel routes, facilitating access management and re-configured intersections**
- **Help revitalization of the Main Street commercial corridor**

# Key Issues

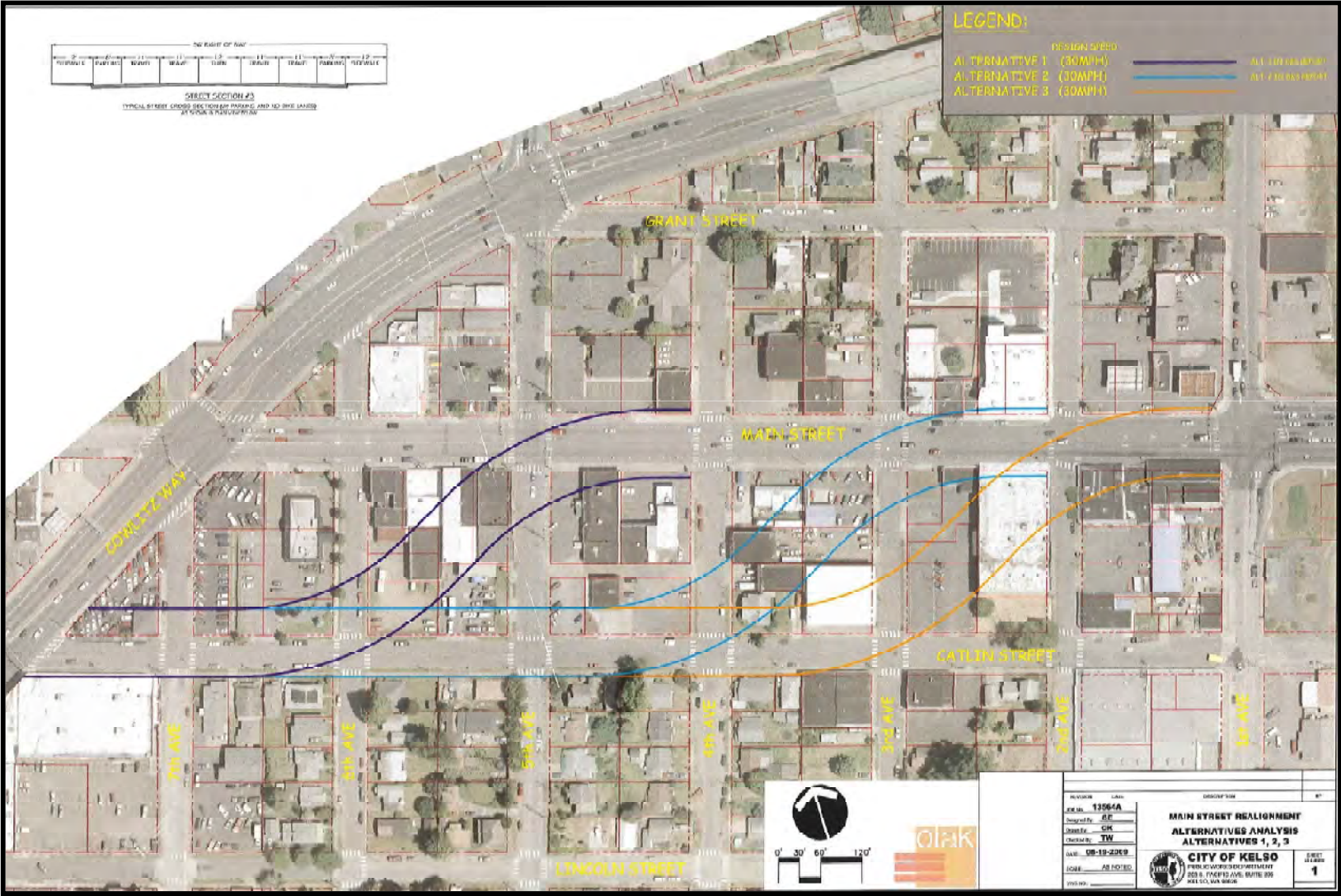
- **Lane and intersection configuration**
- **Pedestrian circulation / bicycle routes**
- **Urban design features that support redevelopment**
- **Design speed**
- **Access management**
- **Property impacts**
- **Parking**
- **Accommodating truck traffic**
- **Fairgrounds access**

# Givens

- **Alternatives will be based on previous study criteria**
- **The realignment provides a crossover segment between Main Street and Catlin Street**
- **Improvements will be limited to those needed to build the realignment**
- **Any “off-corridor” enhancements will be part of a future capital improvement plan for the area**
- **The realigned roadway will be classified as a Principal Arterial**
- **Final decision making authority is the responsibility of the Kelso City Council by way of adoption of the Corridor Plan**



# Realignment Alternatives



# Description of Alternatives

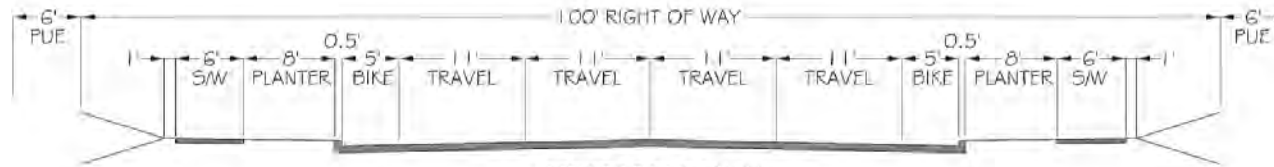
- Alternative 1 (West) – crossover between 4<sup>th</sup> and 6<sup>th</sup>
- Alternative 2 (Central) – crossover between 3<sup>rd</sup> and 5<sup>th</sup>
- Alternative 3 (East) – crossover between 1<sup>st</sup> and 3<sup>rd</sup>

# Key Questions

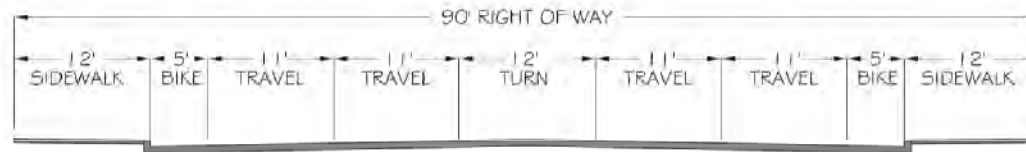
- Preservation of existing commercial corridor
- Location of “crossover”
- Location of new right-of-way on Catlin (north or south side - magnitude of impact to residential or commercial properties)
- Inclusion of parking
- Realignment of the intersection at Cowlitz Way
- Level of urban design investment



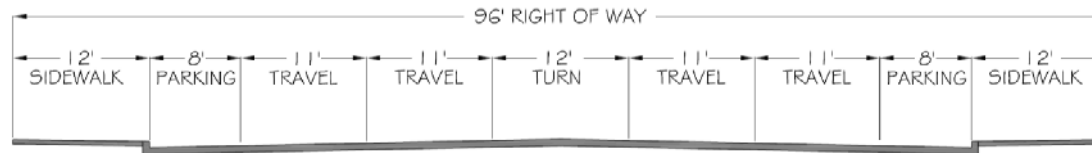
# Typical Sections



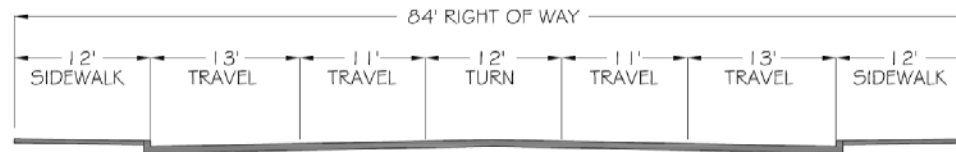
**STREET SECTION #1**  
 CITY OF KELSO MAJOR ARTERIAL  
 FIGURE 3-2 FROM CITY OF KELSO  
 ENGINEERING DESIGN MANUAL  
 (REVISED MAY 2009)



**STREET SECTION #2**  
 TYPICAL STREET CROSS SECTION (W/ BIKE LANES AND NO PARKING)



**STREET SECTION #3**  
 TYPICAL STREET CROSS SECTION (W/ PARKING AND NO BIKE LANES)



**STREET SECTION #4**  
 TYPICAL STREET CROSS SECTION (W/ NO BIKE LANES AND NO PARKING)

# Conceptual Cross Sections Urban Design Opportunities

Section 1



Section 2



Section 3



Section 4



## Conceptual Cross Sections

## Organizing the Pedestrian Streetside Space



Area along the right-of-way that functions to create space between the building facade, wall, or fence and the through zone of the sidewalk.

Obtain the space for clear pedestrian through travel that is often the primary walking area of the sidewalk.

Provide a buffer space between the sidewalk and the adjacent through zone.

Interface between the sidewalk, on-street parking or other adjacent travel lane.

**Pedestrian Buffer Zones**  
The Furnishing, Through, Furnishing, and Edge zones of the pedestrian realm should be designed to facilitate and support the correct and efficient use of the street.

## Urban Design Opportunities

### Urban Design Program

#### Streetscape Opportunities

- Streetscape and urban design treatments define a street's character and identity.
- A cohesive streetscape pattern of furnishings, special paving, street trees, and landscaping provides visual continuity, enhances aesthetics and encourages pedestrian activity.
- Landscaped and/or handicapped medians should be provided to reduce the perceived width of the street (traffic calming) and provide pedestrian refuge space.
- Design driveways for good pedestrian accessibility and with standard widths and configurations.
- Provide street trees with columnar or upright branching form.
- Landscaping should be attractive, easy to maintain and resistant to urban traffic.
- Provide places for people to rest and socialize along the street.
- On-street parking buffers pedestrians from traffic and enhances business.

#### Public Space Opportunities

- The alignment of the street will create opportunities for small pocket parks and mini plazas along the corridor, as well as urban renewal of reconfigured blocks.
- Formal merges can also become focal points along the street, and places for gateway treatments, wayfinding signs, and public art installations.
- The new public spaces will provide pedestrian refuge and crossing opportunities, strengthening neighborhood connectivity.

#### Pedestrian Opportunities

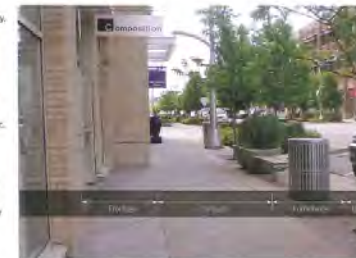
- Provide a continuous, "through" designated walking area, clearly defined and separated from traffic. The pedestrian travel way should never be less than 6 feet wide, not including street trees, plantings, or furnishings (see streetscape or pedestrian zones illustrations).
- Enhance pedestrian lighting, particularly at crossings.
- Provide curb extensions/sloped curbs at intersections and mid-block crossings to shorten the crossing distance and increase visibility of pedestrians.
- Provide frequent pedestrian crossing facilities (striped or distinctively paved crosswalks, pedestrian actuated signals, etc.).
- Strengthen connections along and across the street to neighborhood destinations.

#### Bicycle Opportunities

- Provide minimum 5-foot-wide bicycle lanes, marked within the right-of-way, or route bicycle access down an adjacent alternative street.

#### Gateways and Wayfinding Opportunities

- Enhance the sense of arrival into Kelso with gateway design treatments.
- Provide vehicle-oriented directional signs to aid motorist wayfinding.
- Provide pedestrian oriented signing and kiosks at key locations.
- Enliven public spaces with public art and other design features.



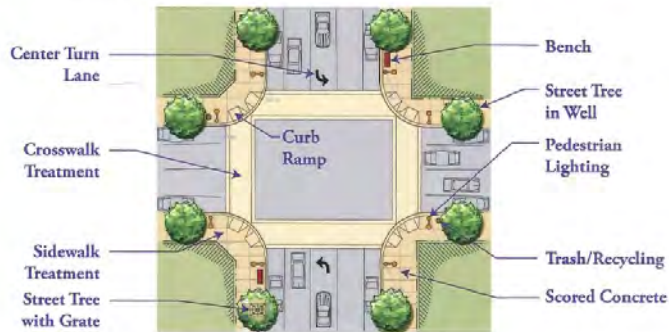
Option with Bike Lanes

# Intersection Design Elements

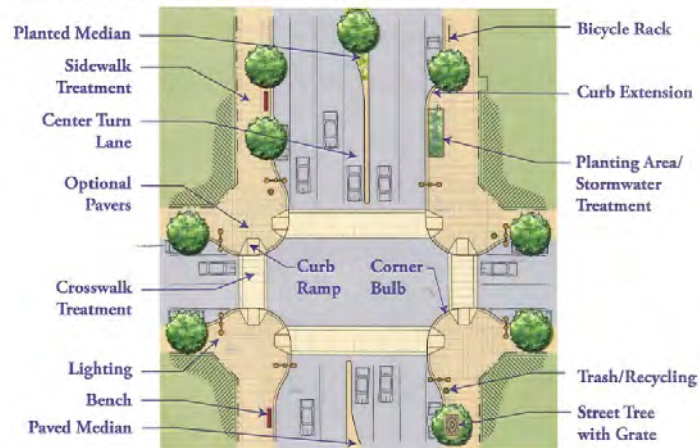
## Intersections Medians Sidewalks

### Intersection Design Elements

INTERSECTION WITHOUT CORNER BULBS



INTERSECTION WITH CORNER BULBS AND MEDIAN



Corner Bulb/Curb Extension with Landscaping



### Intersections Medians Sidewalks

Planted Pedestrian Refuge at Intersection



Stamped Asphalt



Concrete Median



Colored Concrete



Median with Flowering Trees



Striped Crosswalk



Paved Corner Bulb/Curb Extension





# Park / Plaza Treatments Gateways / Public Art

Plaza with Seating



## Park/Plaza Treatments

Plaza with Water Feature



Pocket Park



Mini Plaza



Farmers' Market



Seating Area



Public Art



## Gateways/ Public Art

Gateway Monument



Bike Locker as  
Public Art



Gateway



Public Art  
as Gateway



Sculpture



Clock



Dancing Down  
the Street



# Sidewalk Treatments Furnishings

Street Trees in Wells



## Sidewalk Treatments

Street Trees with Grates



Brick Pavers and Concrete



Pavers



Raised Planter



Planting Strip / Street Trees



Planted Tree Well



Metal Bench



Wood Bench



Decorative Bench and Trash



## Furnishings

Triangle Rack



Decorative Rack



Bicycle Locker



Covered Bicycle Parking



Hanging  
Baskets



Recycling  
Container



Concrete Trash  
Receptacle



Information  
Kiosk





# Lighting Stormwater Treatments

Lighting on Planted Median with Hanging Baskets



## Lighting

Cobra Style Light with Banner



Flow-through Planting Area in Sidewalk



## Stormwater Treatments

Decorative Light Pole with Banner



Pedestrian Scale Lighting: Shepard's Crook Style



Pedestrian Scale Lighting: Single Acorn Style



Stormwater Treatment Area in Action



Stormwater Planting Area in Curb Extension



Stormwater Rain Garden



Stormwater Rain Garden



# Cost Estimate

## West Main Street Realignment Comparative Budget Summary - Concept Level

October 29, 2009

|   | Alternative 1 (West)                | Alternative 2 (Central)             | Alternative 3 (East)                |
|---|-------------------------------------|-------------------------------------|-------------------------------------|
| <b>Typical Street Cross Section</b>           |                                     |                                     |                                     |
| Design and Construction                       | \$7,500,000 to \$8,000,000          | \$7,500,000 to \$8,000,000          | \$7,800,000 to \$8,300,000          |
| Right-of-Way/Land Acquisition                 | \$3,500,000 to \$4,000,000          | \$3,500,000 to \$4,000,000          | \$4,000,000 to \$4,500,000          |
| <b>Total Typical Street Cross Section</b>     | <b>\$11,000,000 to \$12,000,000</b> | <b>\$11,000,000 to \$12,000,000</b> | <b>\$11,800,000 to \$12,800,000</b> |
| <b>Possible Enhancements</b>                  |                                     |                                     |                                     |
| Add Plaza/Park Treatments                     | \$275,000 to \$325,000              | \$275,000 to \$325,000              | \$275,000 to \$325,000              |
| Add Decorative Street Lighting                | \$250,000 to \$350,000              | \$250,000 to \$350,000              | \$250,000 to \$350,000              |
| Add Intersection, Median, Sidewalk Treatments | \$150,000 to \$200,000              | \$150,000 to \$200,000              | \$150,000 to \$200,000              |
| Add Street Furnishings                        | \$75,000 to \$100,000               | \$75,000 to \$100,000               | \$75,000 to \$100,000               |
| Add Public Art/Gateway Treatments             | \$150,000 to \$250,000              | \$150,000 to \$250,000              | \$150,000 to \$250,000              |

**Note:**

Costs are in 2009 dollars.

Based on Alternative Plans 1 - 3, dated 10/29/09.

Costs are for budget comparison purposes only and are not based on detailed design.



# Public Outreach Elements

- Stakeholder / Technical Advisory Committee (STAC)
- Open houses
- Stakeholder / property owner interviews
- City Council
- Project information updates, websites



# Project Information Sheet



## West Main Street Realignment Project

August 2009

### Overview

The City of Kelso is moving forward with developing a corridor plan for realigning West Main Street between the Allen Street Bridge and Ocean Beach Highway. This area has been the subject of several recent studies aimed at removing the "disconnect" between the Allen Street Bridge and Ocean Beach Highway. This disconnect has contributed to the following issues within the corridor:

- Increased congestion;
- Decreased safety for motorists and pedestrians; and
- An underutilized commercial corridor along Main Street.

The realignment project will refine previous studies and result in corridor improvements that will:

- Realign Main Street to meet Ocean Beach Highway;
- Reduce traffic congestion at major corridor intersections;
- Create a western gateway feature for Kelso with an attractive, cohesive boulevard streetscape that improves pedestrian safety;
- Facilitate future transitional zoning compatible with the City's Comprehensive Plan;
- Encourage a network of parallel routes, facilitating access management and re-configured intersections; and
- Provide opportunities for revitalization of the commercial corridor along Main Street.



### Project Vision

The realignment of Main Street between the Allen Street Bridge and Ocean Beach Highway will reduce congestion between Kelso and Longview, improve safety and mobility for all modes of transportation, and create opportunities for revitalizing West Kelso.

### Objectives

The primary objectives of this project will be to build upon previous studies in the selection of a preferred alternative and preparation of a corridor plan as the foundation for advancing the project to preliminary and final design. The corridor plan will define the conceptual roadway improvements for the realignment of West Main Street. This includes affected adjacent streets, stormwater improvements, right of way impacts, and a phasing plan. The corridor plan will provide a recommendation to the City Council for adoption and authorization to proceed to design, with the subsequent preparation of engineered plans for the first or several construction projects.

### Public Involvement

Ongoing coordination with a variety of public agencies and community organizations is an important part of the study process. The City of Kelso will approach public involvement and outreach early and throughout the process in an effort to be responsive to the local community and governmental concerns.

[www.kelso.gov](http://www.kelso.gov)

In addition, members of the City of Kelso and consultant team will be meeting with representatives from local and state governments as well as private and public entities potentially affected by the study. A Stakeholder / Technical Advisory Committee (STAC) has also been formed to provide a forum for community and local agency involvement.

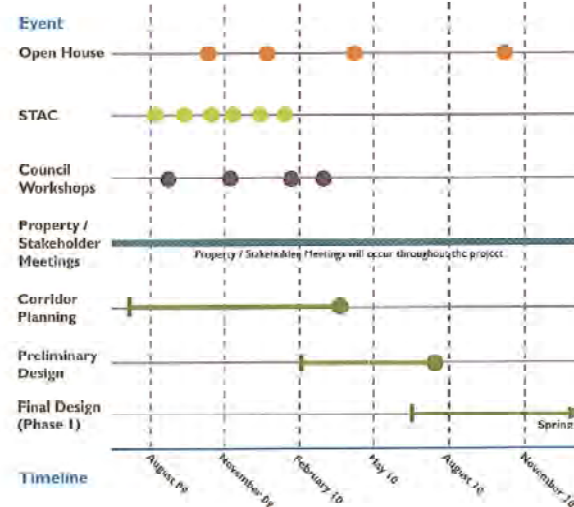
An estimated schedule is shown below. As indicated above, public involvement opportunities and project information will be available throughout development of the project, through a number of means, including:

- Public open houses
- Project information sheets
- Project website updates ([www.kelso.gov](http://www.kelso.gov))
- Regular meetings with the STAC (committee of local stakeholders and agency officials)
- Workshops with City Council members
- One-on-one and focus group meetings with business and property owners along the West Main Street corridor

### Project Funding

The project has received an allotment of \$1.8 million of federal funding under the High Priority Project program. An important facet of this phase of project development is identifying and securing additional project funding to complete the project.

### West Main Street Realignment Project Schedule



### For More Information

If you have questions about the project or would like to provide comments, please contact Al King at (360) 436-2837 or [WestMain@Kelso.Gov](mailto:WestMain@Kelso.Gov). Please visit the project website ([www.kelso.gov](http://www.kelso.gov)) for the latest on information, public event dates and locations.

[www.kelso.gov](http://www.kelso.gov)



# Open House / Comment Form



## West Main Street Realignment Project

### Join Us for an Open House

Thursday, October 29, 2009  
6:00 - 8:00 pm

**Kelso City Hall**  
**City Council Chambers Room**  
**203 South Pacific**  
**Kelso, Washington 98626**

**Event Schedule**

- 6:00 pm Presentation and Q&A
- 6:30 pm Open House

#### Overview

The City of Kelso is developing a corridor plan for realigning West Main Street between the Allen Street Bridge and Ocean Beach Highway. This area has been the subject of several studies aimed at removing the "disconnect" between the Allen Street Bridge and Ocean Beach Highway. The realignment project will refine previous studies and result in corridor improvements that will:

- Realign Main Street to meet Ocean Beach Highway
- Reduce traffic congestion at major corridor intersections
- Create a western gateway feature for Kelso with an attractive, cohesive boulevard streetscape that improves pedestrian safety
- Provide opportunities for revitalization of the commercial corridor along Main Street



If you have questions about the project or would like to provide comments, please contact Al King at (360) 438-2837 or [WestMain@Kelso.Gov](mailto:WestMain@Kelso.Gov). Please visit the project website ([www.publicworks.kelso.gov](http://www.publicworks.kelso.gov)) for the latest on information, public event dates and locations.



PLEASE PROVIDE YOUR COMMENTS

## WEST MAIN STREET REALIGNMENT PROJECT

Thank you for participating in our Open House. After reviewing the exhibits and talking to the Project Team, please help us develop a quality design by responding to a few quick questions.

A variety of streetscape improvements are being considered for the W. Main Street Realignment project. Of the improvements listed below, please pick the most important top five, and rank them with a 1, 2, 3, 4, or 5 (with the number 1 being the most important to you, and the number 5 as the least important).

|   |   |  |
|---|---|--|
| Wider sidewalks                                 | Benches   | Pedestrian crosswalks/crossing improvements  |
| Pedestrian-scale lighting (smaller light poles) | Receptacles for trash and recyclables             | Bicycle racks  |
| Street trees                                    | Public art  | On-street parallel parking   |
| Special paving (pavers or paving accents)       | Pocket parks/mini-plazas (places to sit and talk) | Landscaping/green areas  |
| Signs and information kiosks (wayfinding)       | Gateway to Kelso feature                          | Curb extensions/corner bulbs (helps shorten crossing distance/improve pedestrian visibility) |
| Flower baskets                                  |   |  |

Please check which of the three items are most important to you:

- Minimizing maintenance
- Durability/longevity of street improvement features
- Sustainable/ecological approaches and materials ("green" treatments)
- Creating a distinctive identity for the neighborhood
- Minimizing cost
- Enhancing businesses/economic development
- Improving aesthetics/creating an attractive streetscape

Is there anything else you'd like the Project Team to know about the West Main Street realignment alternatives presented or the design of this project?

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THANK YOU FOR YOUR INPUT.



PLEASE LEAVE YOUR COMMENT FORM IN THE BASKET ON THE WAY OUT OF THE OPEN HOUSE OR DROP IT IN THE MAIL.





# Alternatives Evaluation Criteria

|                                 |   |  |  |                                     |  |
|---------------------------------|---|--|--|-------------------------------------|--|
| <b>Community Considerations</b> | <b>Potential Relocations/Acquisitions:</b><br>- <i>Business Properties</i><br>- <i>Residential Properties</i><br>- <i>Other</i> | <b>Functional Considerations/Traffic and Circulation</b> | <b>Traffic Circulation</b>                     | <b>Environmental Considerations</b> | <b>Stormwater Treatment/Water Quality</b>          |
|                                 | <b>Strengthens Sense of Community;<br/>Creates a Sense of Place (Urban Design)</b>  |  | <b>Pedestrian Mobility/Safety/Connectivity</b> |                                     | <b>Impact to Historical Structures</b>             |
|                                 | <b>Open Space Opportunities</b>   |  | <b>Bicycle Mobility/Safety/Connectivity</b>    |                                     | <b>Impact to Parks and Recreational Areas (4f)</b> |
|                                 | <b>Level of Community/Public Support</b>  |  | <b>Intersection Alignment</b>                  |                                     | <b>Air/Noise</b>                                   |
|                                 | <b>Economic Development Opportunities</b>   |  | <b>On-Street Parking</b>                       |                                     | <b>Hazardous Materials</b>                         |
|                                 | <b>Constructability/Phasing</b>   |  | <b>Environmental Justice</b>                   |                                     |  |
|                                 |   | <b>Cost Considerations</b>                               | <b>Conceptual Cost Estimates</b>               |                                     |  |
|                                 |   |  | <b>Additional Funding Opportunities</b>        |                                     |  |



# Schedule

